

**NOTES:**

1. No lap splicing allowed on the longitudinal rail reinforcing. Splicing shall be staggered.
2. For electrical details, see Standard Plans ES-9A, ES-9B, ES-9C, ES-9D and ES-9E. See Project Plans for electrical layout.
3. For typical metal railing connection details not shown, see Standard Plans A77K1 and A77K2.
4. A maximum of five - 4" and a minimum of two - 4" round openings for future utilities. Openings are to be sealed at ends and extended 8" minimum past end of sidewalk if not used. Duct forms are to be tied down. Minimum of 6" from face of rail to utility opening.
5. Chain link railing is not allowed on Type 80SW Barrier.
6. Walls are to be backfilled before railing is placed.
7. Terminate all longitudinal curb, sidewalk, and deck reinforcement in standard 90° hooks.
8. Dimensions will vary with cross slope and with certain thickness of surfacing.
9. Expansion joint to match deck joint, see Standard Plan B11-63 for expansion joint details.
10. This barrier is to be used only for speeds of 45 MPH or less. For speeds greater than 45 MPH, pedestrians should be protected by a separation traffic barrier.

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET TOTAL SHEETS

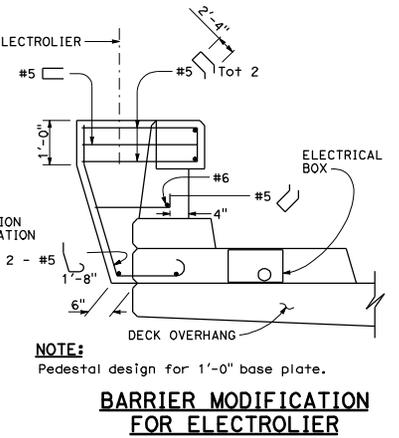
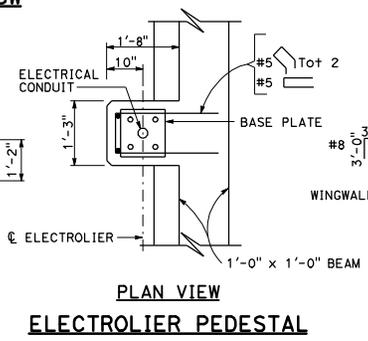
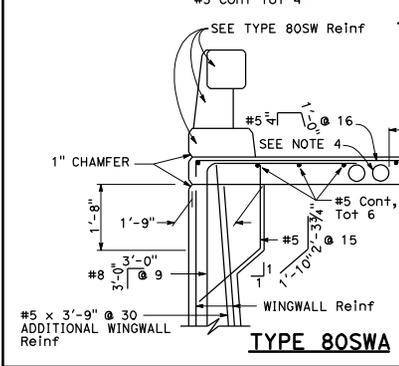
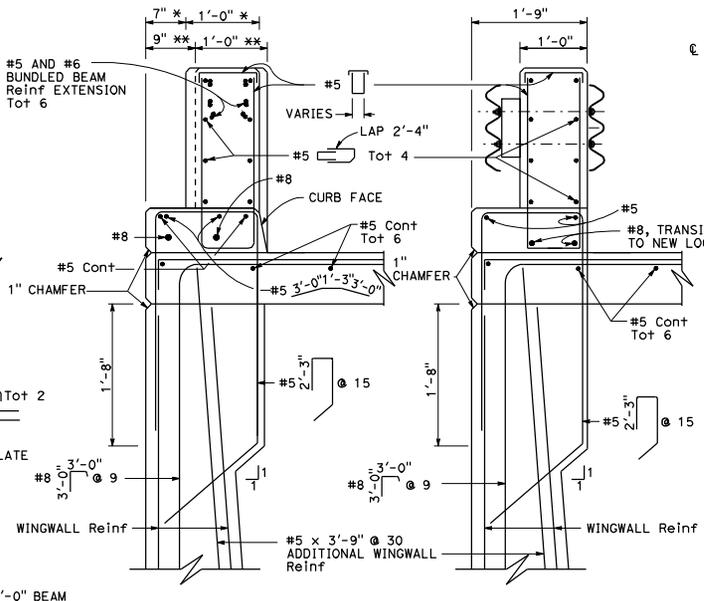
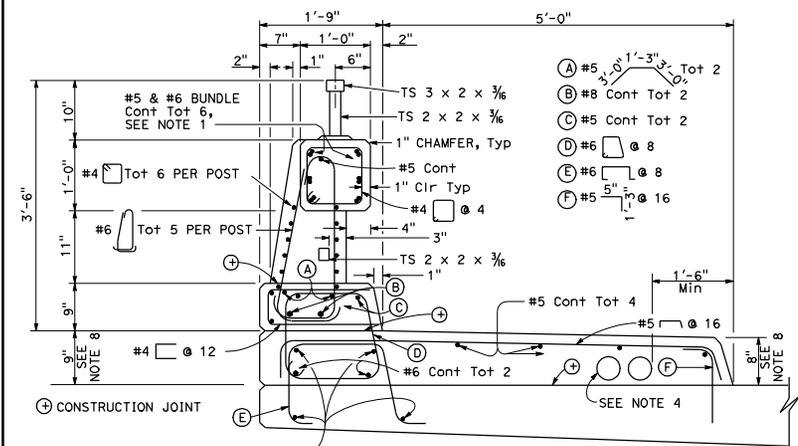
REGISTERED CIVIL ENGINEER

May 20, 2011

PLANS APPROVAL DATE

THE STATE OF CALIFORNIA ON ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

REGISTERED PROFESSIONAL ENGINEER  
Tillot Satter  
No. C42892  
Exp. 3-31-12  
CIVIL  
STATE OF CALIFORNIA



STATE OF CALIFORNIA  
DEPARTMENT OF TRANSPORTATION  
**CONCRETE BARRIER  
TYPE 80SW  
(SHEET 1 OF 3)**

NO SCALE