

NOTES:

- Line post, blocks and hardware to be used are shown on Standard Plans A77A1, A77A2, A77B1, A77C1 and A77C2.
- Guard railing post spacing to be 6'-3" center to center, except as otherwise noted.
- Except as noted, line posts are 6" x 8" x 6'-0" wood with 6" x 8" x 1'-2" wood blocks. W6 x 9 steel posts, 6'-0" in length, with 6" x 8" x 1'-2" notched wood blocks or notched recycled plastic blocks may be used for 6" x 8" x 6'-0" wood line posts with 6" x 8" x 1'-2" wood blocks where applicable and when specified.
- A 4'-0" minimum clearance is required between the face of the railing and the face of a fixed object located directly behind standard guard railing section with post spacing of 6'-3". Construct guard railing as shown in the detail "Strengthened Railing Sections for Fixed Objects" on this plan, where the clearance between the face of the railing and the face of a fixed object is less than 4'-0", but not less than 2'-3". Where the clearance is less than 2'-3", a concrete wall or barrier should be constructed to shield the fixed object(s).
- Direction of adjacent traffic indicated by → .
- For End Anchor Assembly (Type SFT) details, see Standard Plan A77H1.
- Type of crash cushion to be used will be shown on the Project Plans.
- Type 15A layout is typically used on multilane freeways or expressways to shield fixed objects in the area between separated one-way roadbeds.
- For typical flare offsets for 25'-0" length parabola with maximum offset of 1'-0", see Standard Plan A77E1.
- The 15:1 or flatter flare is measured off of the edge of the traveled way.
- W6 x 15 steel post, 8'-0" in length, with 8" x 8" x 1'-2" notched wood block or notched recycled plastic blocks may be used in place of the 10" x 10" x 8'-0" wood post with 8" x 8" x 1'-2" wood block shown in the "Strengthened Railing Sections Detail".

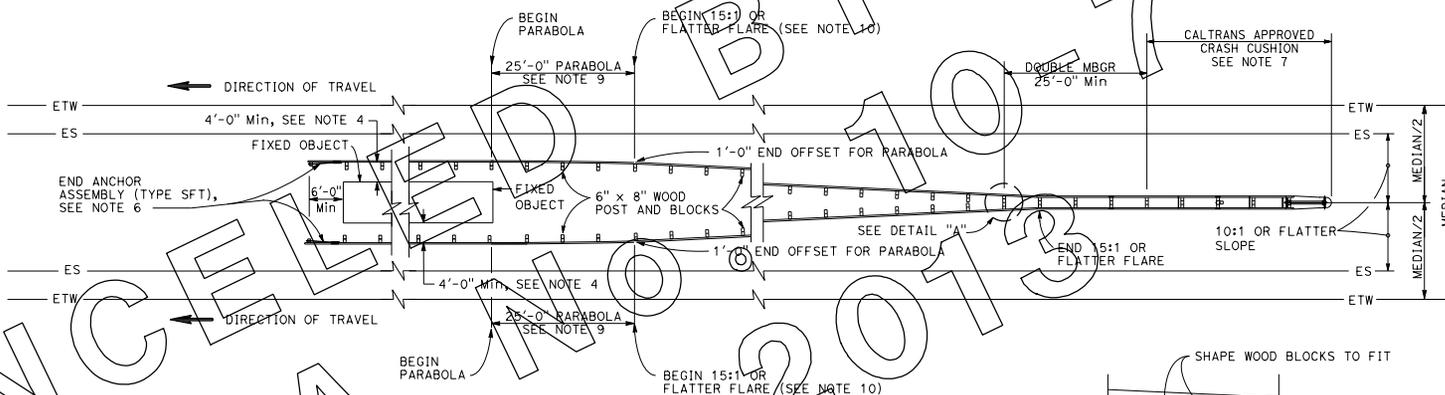
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS

Randell D. Hiatt
REGISTERED CIVIL ENGINEER

May 20, 2011
PLANS APPROVAL DATE

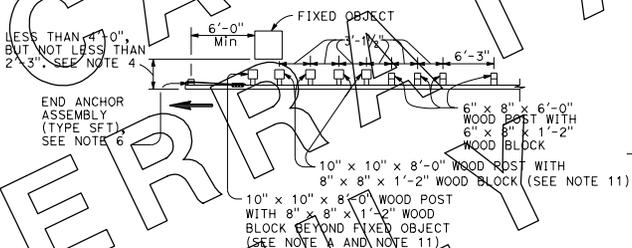
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REGISTERED PROFESSIONAL ENGINEER
No. C60200
Exp. 6-30-11
CIVIL
STATE OF CALIFORNIA



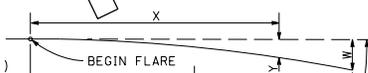
TYPE 15A LAYOUT

See Note 8



STRENGTHENED RAILING SECTIONS FOR FIXED OBJECT

Use strengthened railing sections with Type 15A layout where minimum clearance between the face of the guard railing and the fixed object(s) is less than 4'-0", but not less than 2'-3". See Note 4.

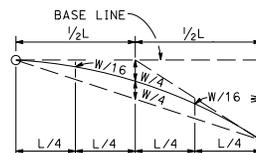


BASE LINE (EDGE OF PAVED SHOULDER OR OFFSET LINE OF EDGE OF TRAVELED WAY)

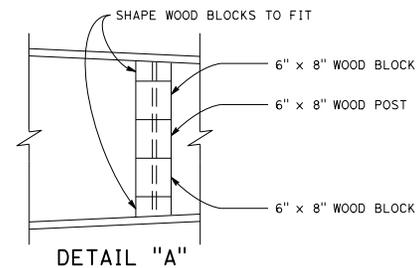
Y = OFFSET FROM BASE LINE
W = MAXIMUM OFFSET
X = DISTANCE ALONG BASE LINE
L = LENGTH OF FLARE

$$Y = \frac{WX^2}{L^2}$$

PARABOLIC FLARE OFFSETS



TYPICAL PARABOLIC LAYOUT



DETAIL "A"

STATE OF CALIFORNIA
DEPARTMENT OF TRANSPORTATION

**METAL BEAM GUARD RAILING
TYPICAL LAYOUTS FOR
FIXED OBJECTS
BETWEEN SEPARATE ROADBEDS
(ONE-WAY TRAFFIC)**

NO SCALE

A77G2

2010 STANDARD PLAN A77G2